

Divisions affected: *Chalgrove and Watlington*

CABINET MEMBER FOR HIGHWAY MANAGEMENT – 13 OCTOBER 2022

CHALGROVE – PROPOSED 20MPH SPEED LIMIT

Report by Corporate Director, Environment and Place

RECOMMENDATION

1. The Cabinet Member for Environment is RECOMMENDED to approve as advertised
 - a. the 20mph speed limit at Chalgrove,
 - b. the extended 20mph on Oxford Way, as well as the new roads lying north & south of Oxford Way forming the residential development located to the west of Marley Lane.

Executive summary

2. This report presents the responses to a consultation on a proposal to introduce a 20mph speed limit in Chalgrove on those roads lying south of the B480, replacing the existing 30mph speed limits in their entirety. It is though proposed that the roads lying to the north of the B480 in the vicinity of the airfield & Monument Park will remain 30mph. The proposal is shown in **Annex 1**.

Financial Implications

3. Funding for consultation on the proposals has been provided by the County Councils 20mph speed limit project.

Equality and Inclusion Implications

4. No implications in respect of equalities or inclusion have been identified in respect of the proposals.

Sustainability Implications

5. The proposals would help facilitate walking and cycling within the village and the safe movement of traffic.

Consultation

6. Formal consultation was carried out between 04 August and 02 September 2022. A notice was published in the Oxford Times newspaper, and an email sent to statutory consultees & key-stakeholders, including Thames Valley Police, the Fire & Rescue Service, Ambulance service, Bus operators, countywide transport, access & disabled peoples user groups, South Oxfordshire District Council, the local District Cllr, Chalgrove parish council, and the local County Councillor representing the Chalgrove & Watlington division.
7. 13 responses were received via the online consultation survey during the course of the formal consultation, and these are summarised in the table below.

Proposal	Object	Concerns	Support	No opinion/ objection	Total
Chalgrove 20mph	1	0	12 (92%)	0	13

8. Those who responded online, were also asked whether if the 20mph speed limit proposals were implemented, would it likely influence a change to their mode of travel in the area, the results of which are shown below

Travel Change	Number
Yes – walk/wheel more	3 (23%)
Yes - cycle more	2 (15%)
No	8 (62%)

9. Furthermore, four emails were received, with the local County Councillor representing the Chalgrove & Watlington submitting their support, Thames Valley Police (TVP) raising concerns, Oxford Bus Company & Stagecoach confirming they had no comments in relation to the specific proposals, and one member of the public raising concerns.
10. An additional formal consultation was carried out between 25 August and 16 September 2022 on the proposal to further extend 20mph speed limits in the area. A notice was published in the Oxford Times, and the statutory consultees & key-stakeholders from the original consultation were also emailed with the details.
11. Specifically, these proposals sought to include the entire length of Oxford Way running north-westwards from High Street to the junction with the B480, as well as incorporating the new roads lying north & south of Oxford Way forming the residential development located to the west of Marley Lane (as shown in **Annex 2**). These proposals were put forward as a direct response to the adjacent residential development.
12. Three emails were received during the course of this consultation, with Thames Valley Police not objecting, but raising the same concerns from the previous

consultation for the wider Chalgrove proposals, Oxford Bus Company objecting to the inclusion of Oxford Way section between the B480 and Marley Lane, and Stagecoach Bus Company confirming they had no comments as a result of not being the operator of bus services through the village.

13. The responses are shown at **Annex 3**, and copies of the original responses are available for inspection by County Councillors.

Officer response to objections/concerns

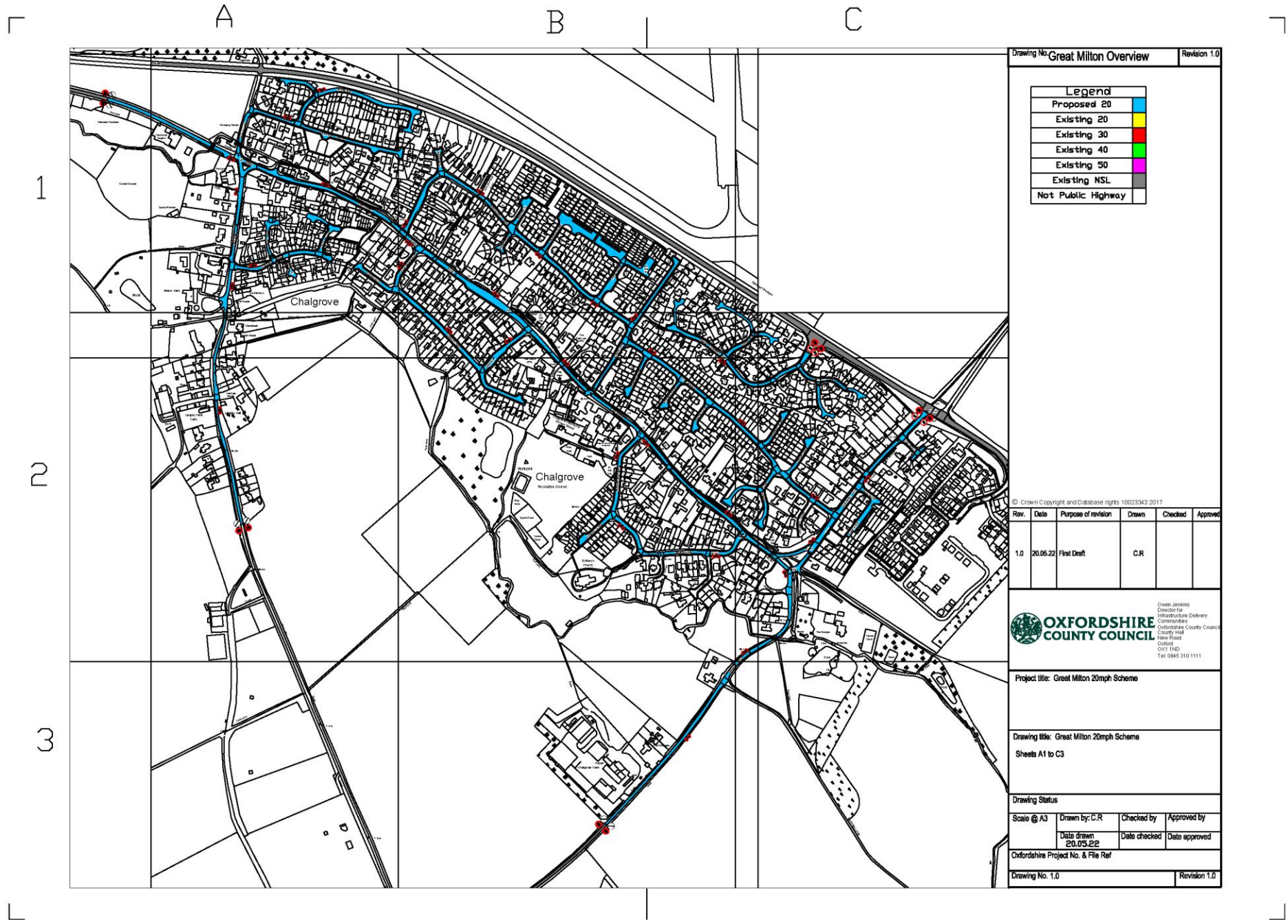
14. Thames Valley Police expressed no objection to the proposals but confirmed their stance that primarily 20 mph speed limits and zones should be self-enforcing. They welcomed the engineering measures that are included and suggested further measures should not be dismissed.
15. The objections received from the bus company stated that “The inclusion of the section between the B480 and Marley Lane is not acceptable to us as this is a section of over 900m in length which is used by our 11 bus service”. They have also stated that they may seek to reduce the frequency of the 11 bus due to the impact of the lower speed over a 900m length. As there is a current interaction between vulnerable road users (and a future one due to planned growth in the area) it is considered by officers that a 900m length will not have a significant impact on the bus journey times. Other objects stated that 20mph restrictions should be targeted at school/park areas and that funding should be spent on VMS instead as it is a more effective way of slowing vehicular speeds.
16. Officers will work with the bus company to review this section of the proposal if it is agreed as recommended to assess the impact on the journey time. Should it become an issue officers will review this section of the network and consider reverting back to 30mph (along the 900m section). The priority for the county council and the Parish is being given to safety of residents.
17. It should be noted that the county council is changing its approach and acceptance of 20mph restrictions, which will enable a much greater coverage across Oxfordshire.
18. The proposal is aimed to make streets safer, encourage residents to walk or cycle & reduce noise and pollution. The consultation proposal puts forward a scheme that encompasses the whole village but this extent may be reduced prior to implementation in accordance with your comments and in discussion with the Parish Council. Signing and other measures will as far as possible be sympathetically applied bearing in mind the aesthetic of the village.
19. The Parish Council support the proposals.

Bill Cotton
Corporate Director, Environment and Place

Annexes Annex 1: Consultation Plan
 Annex 2: Consultation Plan (additional element)
 Annex 3: Consultation responses

Contact Officers: Tim Shickle 07920 591545
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October 2022



Drawing No. Great Milton Overview Revision 1.0

Legend	
Proposed 20	■
Existing 20	■
Existing 30	■
Existing 40	■
Existing 50	■
Existing NSL	■
Not Public Highway	■

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Rev.	Date	Purpose of revision	Drawn	Checked	Approved
1.0	20.05.22	Final Draft	C.R		

OXFORDSHIRE COUNTY COUNCIL
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Project title: Great Milton 20mph Scheme

Drawing title: Great Milton 20mph Scheme
 Sheets A1 to C3

Drawing Status

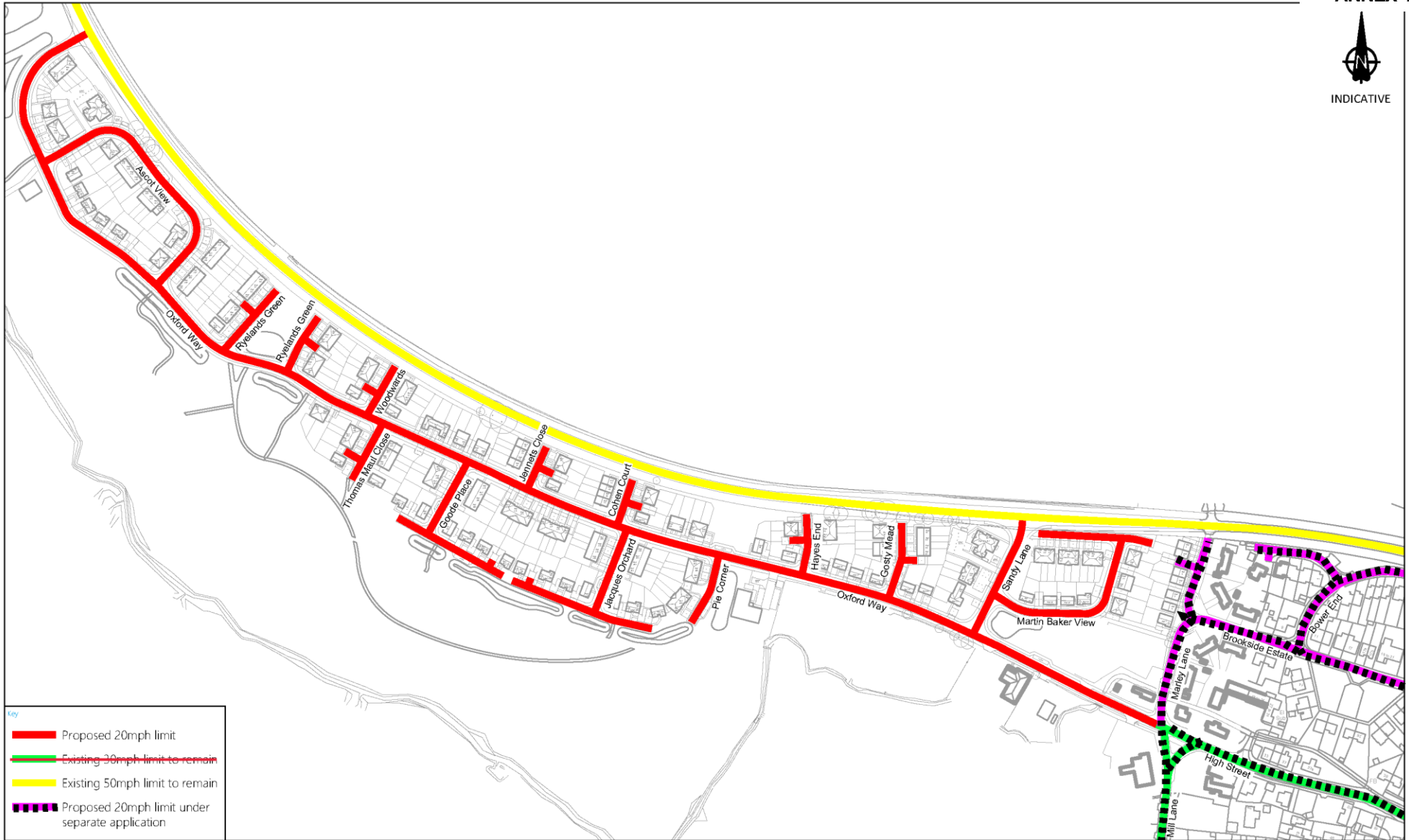
Scale @ A3	Drawn by: C.R	Checked by	Approved by
	Date drawn 20.05.22	Date checked	Date approved

Oxfordshire Project No. & File Ref

Drawing No. 1.0 Revision 1.0



INDICATIVE



Key

	Proposed 20mph limit
	Existing 20mph limit to remain
	Existing 50mph limit to remain
	Proposed 20mph limit under separate application

Notes

- Do not scale from this drawing. All dimensions shown are in metres unless noted otherwise.
- This drawing has been based upon topographical survey information produced by others and Milestone Transport Planning cannot be held responsible for any discrepancies which may arise because of it.

Ordnance Survey Licence number: 100057360

Drawing Revisions

Rev.	Chg.	Date:	Details
-	AM	29/06/2022	First issue
A	AM	30/06/2022	Minor amendments

Clients

Chk: SRT SRT	Ridgepoint Homes Ltd
Project	Land West of Marley Lane, Chalgrove

Title

Traffic Regulation Order Proposed 20mph Limit

MILESTONE
TRANSPORT PLANNING

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Gateshead IBC, Mulgrave Terrace, Gateshead, NE8 1AN
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Drawing Number: 20072/TRO/01	Scale: 1:2500 @ A3
Revisor: A	

RESPONDENT	COMMENTS
(1) Traffic Management Officer, (Thames Valley Police)	<p>Concerns - Thames Valley Police welcome the opportunity to engage on plans for road safety improvement and acknowledge that 20mph limits can be a useful tool in road safety. There are other reasons 20mph limits may be desirable for communities, such as environmental concerns, and creating a shared space environment to encourage greater diversity of road users.</p> <p>Compliance with 20mph limits is a challenging issue as there is a difference between the achievable results of the various available schemes. For example a sign-only scheme will only have a limited effect on the mean speeds, as opposed to other schemes that influence the road environment, which is recognised as being key to achieving compliance. If a speed limit is set too low and is ignored then this could result in the vulnerable road user being less safe. It can also cause a dis-proportionate number of drivers to criminalise themselves and could bring the system of speed limits into disrepute.</p> <p>Thames Valley Police have no policy to enforce based on arbitrary speed limits alone but will enforce based on threat of harm, risk and resourcing. 20mph limits are not excluded from this and will be enforced where appropriate. There should be no expectation that the police would be able to provide regular enforcement if a speed limit is set too low as this could result in an unreasonable additional demand on police resources and there are no additional resources available to support extra enforcement. Messages from partners that police will not enforce need to be discouraged. Such messaging can encourage non-compliance and should be avoided.</p> <p>The policy of Thames Valley Police is to use sound practical and realistic criteria (Setting local speed limits - GOV.UK (www.gov.uk)) when responding to Highway Authorities in an effort to promote consistency and to reduce the burden of constant and unnecessary enforcement. The advice shown in Circular Roads 1/2013 states.</p> <p>The key factors that should be taken into account in any decisions on local speed limits are:</p> <ul style="list-style-type: none"> • history of collisions • road geometry and engineering • road function • composition of road users (including existing and potential levels of vulnerable road users) • existing traffic speeds

	<ul style="list-style-type: none"> • road environment <p>However I recognise Oxfordshire County Council now have their own Policy for Setting Speed Limits and I expect full compliance of that policy going forward in relation to both monitoring , future engineering and self-enforcement through Community Speed Watch .</p> <p>Our stance remains that primarily 20 mph speed limits and zones should be self-enforcing</p> <p>Speed limits should be considered as part of a package of measures to manage vehicle speeds and improve road safety. Changes to the highway (for example through narrowing, providing vertical traffic calming or re-aligning the road) may be required to encourage lower speeds in addition to any change in speed limit. Though these may be more expensive, they are more likely to be successful in the long term in achieving lower speeds without the need for increased police enforcement to penalise substantial numbers of motorists.</p> <ul style="list-style-type: none"> • Early result from the pilot site on the B480 at Cuxham show a reduction in Mean speeds, but an increase in the level of non-compliance by the majority of motorist
(2) Local County Cllr, (Chalgrove & Watlington division)	<p>Support - I welcome this change in Chalgrove to make the village a safer place for local residents. Consideration should be given to how the limits will apply to new housing developments at either side of the village, with the east-side Chalgrove Meadows development mostly complete and occupied.</p>
(3) Oxford Bus Company	<p>Oxford Bus Company was supportive of the original consultation.</p> <p>Object - However, we would like to object to this revised consultation, issued on 25th August 2022. The inclusion of the section between the B480 and Marley Lane is not acceptable to us as this is a section of over 900m in length which is used by our 11 bus service, the only public transport service operating in Chalgrove and one which we believe to be highly valued by the local community in the area.</p> <p>Currently this road is at National Speed Limit and therefore the proposals to reduce to 20mph will have a significantly deleterious impact on bus journey times and will lead to bus services from Chalgrove and Watlington becoming less attractive against other modes, notably private cars which would be able to stay on the B480 and avoid such impact.</p>

	<p>While we understand that a new housing estate is to be built in this area, this section of road - which I believe is to be named Oxford Way - should remain at 30mph and in our view should NOT be reduced to 20mph. Should the county proceed with the proposed reduction to 20mph we would need to consider the impact on the achievability of the timetable for service 11 and we believe this would be highly likely to require reductions in service frequency, should this policy be implemented.</p>
(4) Stagecoach Bus Company	No comments
(5) Local Cllr, (Chalgrove, Mill Lane)	<p>Support – Speeding in the village is an issue</p> <p>Travel change: Yes – cycle more</p>
(6) Member of public, (Chalgrove, Mill Lane)	<p>Object – 20mph zones are indiscriminate, economically costly, increase traffic and offer no positive impact on road safety.</p> <p>The strength in 20mph limits comes from targeting them where they are most needed such as close to schools, parks, and residential areas with high levels of walking and cycling. While 20mph works in certain areas with local support, still need some roads for movement for through traffic, buses, deliveries and here the speed limit should be higher. Chalgrove High Street is on the main bus route (between Cowley and Watlington). It is also used for deliveries to its shops. As for the village school, a zebra crossing would be of more use than a blanket 20mph speed limit the impact of which is wasted. In America the speed limits outside schools work because they are in immediate vicinity and at key times only.</p> <p>Government figures revealed shockingly low compliance with 20mph limits. DfT data shows that in free-flowing traffic areas (without traffic calming measures), 87 per cent of cars exceeded the 20mph limit. So as a policy blanket bans don't work and are just a waste of money.</p> <p>Personally I would much rather see the money being spent on those flashing speed indicators that remind drivers of the speed they should be doing. I understand that the council already has funding for one of these and with the money that will be wasted on this proposed blanket 20mph it would be far better to use it to buy more to position at key places throughout the village</p>

	<p>To quote Luke Bosdet, a spokesman for the AA, argues the over-proliferation of 20mph zones is a blunt tool that will only serve to confuse drivers. “Speed limits work in places where they make sense to drivers, where there is a particular hazard that requires them to slow down,” he says. “The problem is 20mph zones pop up all over the place and they’ve lost their meaning.” This has been proved in Cuxham where after moderate compliance in the first month, things have reverted back. The key benefit there is the speed indicator that does slow traffic down.</p> <p>Chalgrove is a car based community and for the vast majority who live here the only way to get around is to drive. So having a driving licence is essential for employment. For the youngsters the risk of losing their driving licence under the totting up scheme would force them to relocate away from the village. A 20 mph speed limit has consequences, but not positive.</p> <p>The vast majority of the drivers here are sensible and drive carefully so any reduction in the speed limit probably won’t change their behaviour (apart from making them take their eyes off the road to check their speedometer more often) and the idiots remain idiots and will unlikely change.</p> <p>So for various reasons I strongly object to the proposed reduction from 30mph to 20mph. However I would greatly appreciate more speed indicators – always nice to have a reminder</p> <p>Travel change: No</p>
<p>(7) Member of public, (Chalgrove, High Street)</p>	<p>Concerns - there are equally pressing concerns:</p> <ol style="list-style-type: none"> 1 the number of vehicles currently riding the pavement at speed to get past other vehicles which is now a daily occurrence 2 pinch points in the village where vehicles speed to get past a line of cars such as along the High Street 3 unregulated tonnage of construction and other vehicles using the High Street 4 no speed restrictions or signage around the building site at the top end of the village at the slip road entrance which includes the surgery area <p>Imposing a speed restriction without proper monitoring prior to this of traffic flow does not seem sensible although I am in favour of 20mph in general. If it is recommended I can predict that it will be largely ignored. We have children</p>

	<p>learning to cycle from the primary school, elderly in wheelchairs and motorised buggies, horseriders and cyclists using the High Street who need safeguarding. Even crossing the road is tricky. One day I calculated that at least 1000 vehicles use the high Street which I think is excessive. So monitoring of the traffic situation in the village would be a good idea as would monitoring pollution levels created by traffic along the High Street (and I would add subsidence to buildings along the length of it due to the increased number of heavy goods vehicles).</p>
<p>(8) Member of public, (Chalgrove, Sixpenny Lane)</p>	<p>Support - In principle a good idea but not sure it will slow down those that are already speeding through the village.</p> <p>Travel change: No</p>
<p>(9) Member of public, (Chalgrove, High Street)</p>	<p>Support - The High Street is relatively narrow, and has parked cars on both sides; 30mph is already difficult to achieve, and with a Primary School, shops and pubs lining the High Street, it is dangerous to the public to drive at that speed. Most of the roads off the High Street are residential, in many cases relatively narrow, and with parked cars on both sides. 20mph is fast enough in these roads.</p> <p>Travel change: Yes - cycle more</p>
<p>(10) Member of public, (Chalgrove, Willow Mead)</p>	<p>Support - Speed in the village needs to be reduced to protect the public</p> <p>Travel change: No</p>
<p>(11) Member of public, (Chalgrove, Grays Close)</p>	<p>Support - Too many cars racing through the village.</p> <p>Travel change: No</p>
<p>(12) Member of public, (Chalgrove, Grays Close)</p>	<p>Support - With ever more homes being built locally, vehicle and pedestrians numbers are increasing. Any move to reduce the speed of vehicles travelling through the village should be encouraged</p> <p>Travel change: No</p>

<p>(13) Member of public, (Chalgrove, Argosy Close)</p>	<p>Support - Support as there are lots of parked cars now on all roads with limited passing places.. slower the better Travel change: No</p>
<p>(14) Member of public, (Chalgrove, Liddon Road)</p>	<p>Support - Cars are always parked in the road in the High Street which means cars have to give way to each other, it is important that drivers slow down. There is a primary school and 5 shops which are used by all the community including the elderly so it is vital for safety that the maximum speed limit is 20 mph for the safety of everyone crossing the road in the village or driving through it. Travel change: No</p>
<p>(15) Member of public, (Chalgrove, Brinkinfield Road)</p>	<p>Support - As a resident of Chalgrove I am concerned for the safety of my family, both children and elderly parents. Traffic and noise in the village has increased, along with the speed of motorists using certain roads in the village as a cut through. Travel change: Yes – walk/wheel more</p>
<p>(16) Member of public, (Chalgrove, Brinkinfield Road)</p>	<p>Support - Need to slow traffic. Safety of people in village, especially children. Noise. Pollution. Travel change: Yes – walk/wheel more</p>
<p>(17) Member of public, (Chalgrove, Hardings)</p>	<p>Support - Road safety for people and wildlife Travel change: No</p>
<p>(18) Member of public, (Chalgrove Church Road)</p>	<p>Support - 20 is plenty Travel change: Yes – walk/wheel more</p>

